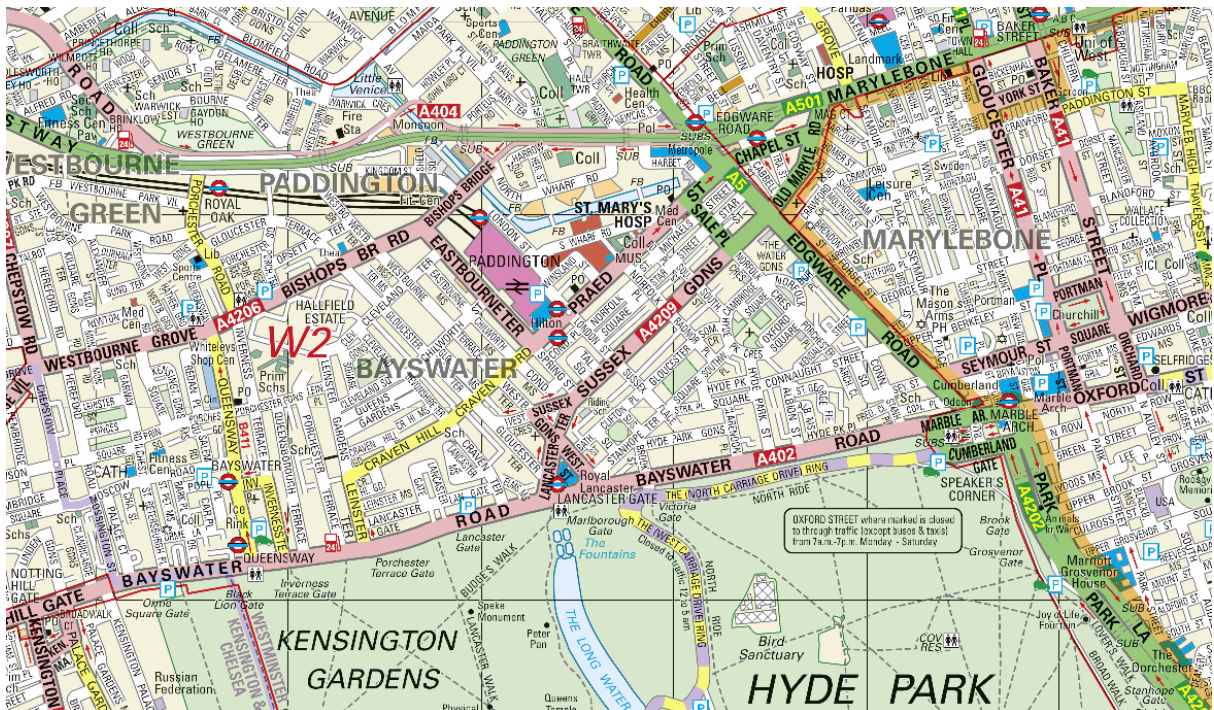




City of Westminster

Paddington Station



The main Paddington station between Bishops Bridge Road and Praed Street was designed by Brunel, who was later commemorated by a statue on Platform 1, by the exit to the former taxi rank. Much of the architectural detailing was by his associate Matthew Digby Wyatt. The station opened on 29 May 1854. The glazed roof is supported by wrought iron arches in three spans, respectively spanning 68 feet (21 m), 102 feet (31 m) and 70 feet (21 m). The roof is 699 feet (210 m) long, and the original roof spans had two transepts connecting the three spans.

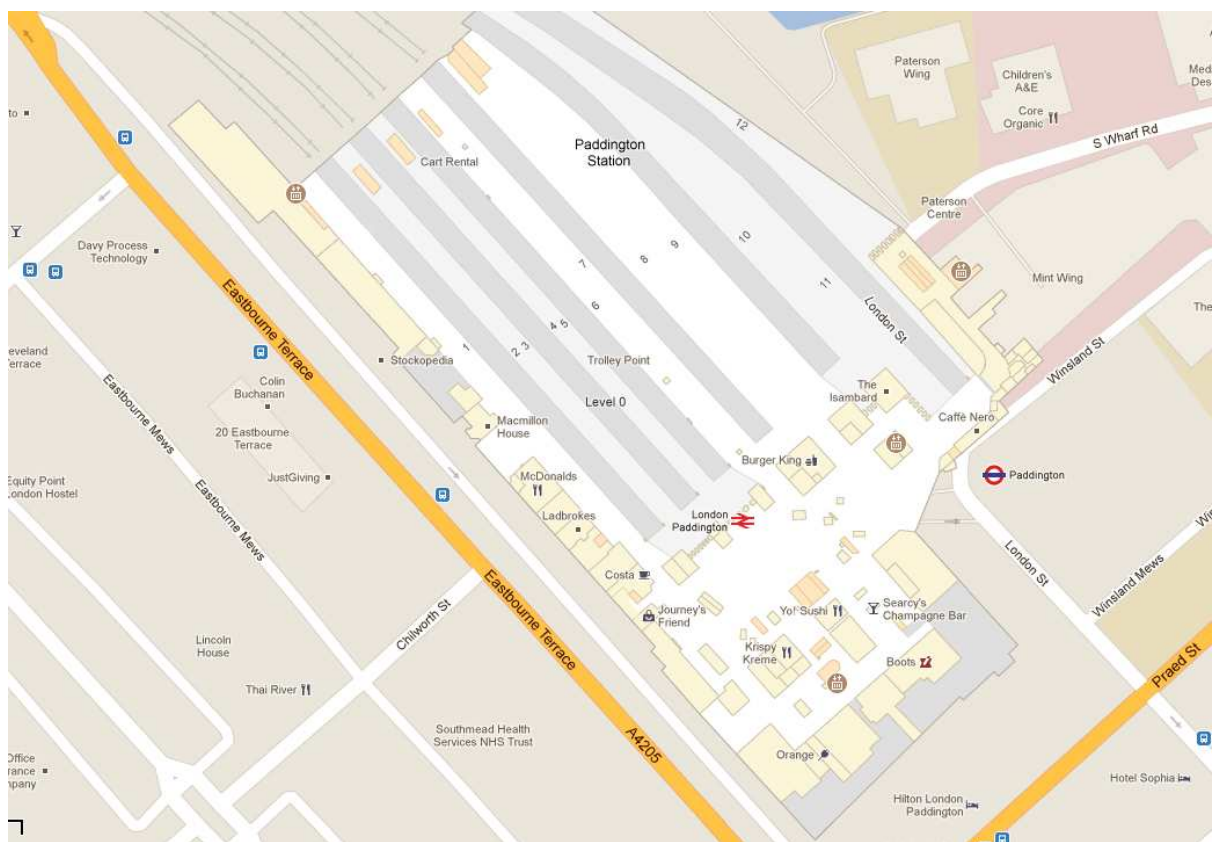


Cross transept in roof, Paddington Station



Paddington has 14 terminal platforms, numbered 1 to 14 from south-west to north-east (left to right as seen from the main concourse). Platforms 1 to 8 are below the original three spans of Brunel's 1854 train shed, and platforms 9 to 12 are beneath the later fourth span. Platforms 13 and 14 are within the Metropolitan Railway's old Bishop's Road (Suburban) station to the north-west. Immediately alongside are two through platforms, numbered 15 and 16, used by the London Underground's Hammersmith & City and Circle lines

Platforms 6 and 7 are dedicated to the Heathrow Express, and platforms 13 and 14 can be used only by the 2- and 3-car Turbo trains used on local services. Platforms 1 to 5 and 8 to 12 can be used by any of the station's train services; however, long-distance trains generally use the south-western platforms, and local trains (including Heathrow Connect) the north-eastern ones.



The station concourse at rush hour.



View of the four arches of the station roof

The station concourse stretches across the head of platforms 1 to 12, underneath the London end of the four main train sheds. Platforms 13 and 14 can only be reached indirectly via the northern-western end of platform 12, or from the footbridge which crosses the north-western end of the station and gives access to all platforms.]

The area between the rear of the Great Western Hotel and the station concourse is traditionally called The Lawn. It was originally unroofed and occupied by sidings, but was later built up to form part of the station's first pedestrian concourse. The Lawn has recently been re-roofed and separated from the concourse by a glass screen wall. It is now surrounded by shops and cafés on several levels.



The Great Western Hotel was built on Praed Street in front of the station in 1851-1854 by architect Philip Charles Hardwick, son of Philip Hardwick (designer of the Euston Arch).

The station was substantially enlarged in 1906-1915 and a fourth span of 109 feet (33 m) was added on the north side, parallel to the others. The new span was built in a similar style to the original three spans, but the detailing is different and it does not possess the transepts of the earlier spans.



Paddington Station: Interior.

Paddington Station in Victorian Times